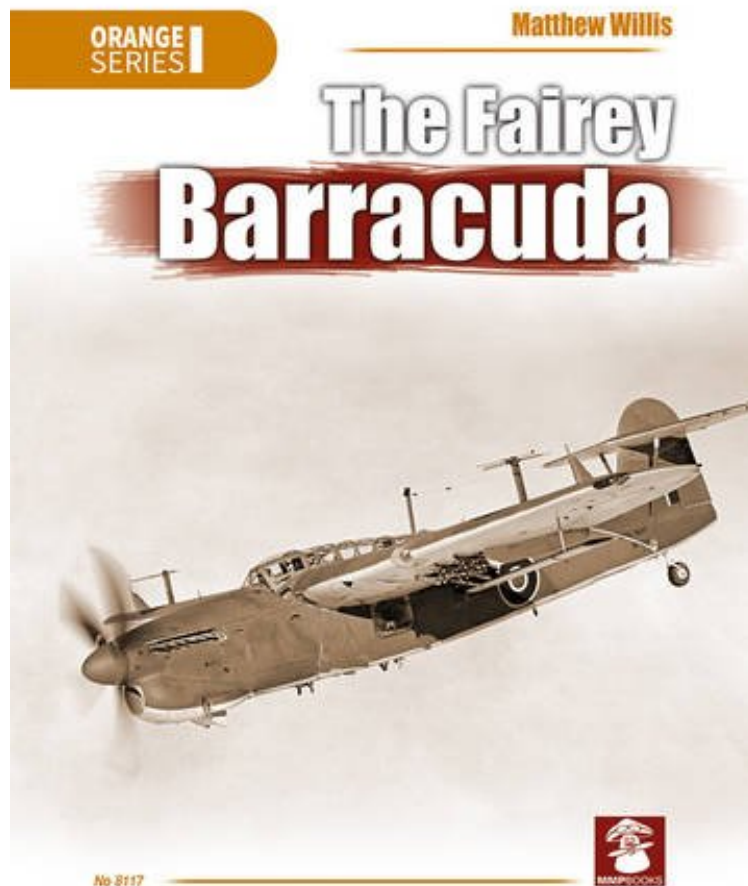


[Get free] The Fairey Barracuda (Orange Series)

## The Fairey Barracuda (Orange Series)

Matthew Willis

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**Matthew Willis : The Fairey Barracuda (Orange Series)** before purchasing it in order to gage whether or not it would be worth my time, and all praised The Fairey Barracuda (Orange Series):

1 of 1 people found the following review helpful. A Microcosm of Typically Insipid Royal Navy Requirements and The ResultBy JCALExcellent book covering the origin, development, production and use of the Barracuda. In the first few pages the author describes the environment the Barracuda was developed in. Frankly, I don't believe he goes far enough.If a mediocre aircraft ever deserved to be examined from a complete perspective of the factors that influenced its development and use, the Barracuda is it.Here are some examples to contrast the British and American production scale of WWII along with some factors somewhat unique to the British;1. Excess production capacity, the Brewster Buccaneer failed,the SB2C was almost a failure. The USN had the TBF/TBM and was building a factory for the

Vought TBY. That's four programs where metal was cut and three programs that reached production! What excess capacity did the British have? 2. The Wichita B-29 program had more engineers working at that one, single plant than England had engineers working in the entire industry. 3. The aircraft of the RN were controlled by the RAF almost until the last moments before WWII, austere budgets, a customer that was reluctant to develop new technology aircraft, obsolete specifications, etc. all conspired to leave the RN bereft of any truly competitive and state of the art aircraft literally until the introduction of the Hawker Sea Fury. Consider the 82% increase in HP over the life of the Barracuda airframe compared to only a 38% increase in empty weight, All of that HP resulted in only a 14% increase in useful load. (The Barracuda also was competing for better engines, all of which went to higher priority programs. - Good examples in the US would include cancellation of the Boeing PBB-1 and the Beech XA-38 Grizzly to ensure availability of the R3350 for those B-29's being built in Wichita.) The exigencies of war combined with a lack of engines and manpower conspired with insipid customer design requirements consigned this aircraft to mediocrity. The fact the RN got the mileage out of it that they did is a real tribute to the men that flew, maintained and supported it. In fact, I will go further and say that in my opinion the Barracuda, Sea Fury, possibly the Wyvern and the Blackburn Buccaneer were the only effective aircraft developed by the Royal Navy after 1935. The RN can't even take credit for the Harrier as that was originally operated by the RAF. The book itself? My only criticisms cover the chapter on technical descriptions, there is no description of the hydraulic system and the other descriptions are rather brief. I will say the book is well illustrated with excellent if low-res quality photographs. A brief perusal of these photographs reveals how the Barracuda appeared to be massively overbuilt. Operational use gets very good coverage with good coverage of the attack on the Tirpitz. Post war use gets some good coverage and the chapter on flying the aircraft appears to be pretty well balanced. There is no index, but there are footnotes which appear to be adequate. Modelers will get some limited use from this book. However, I do not recommend purchase if it is used strictly for modeling details. Highly recommended for those interested in RN Aircraft, British WWII aircraft development and production and RN squadron histories. 3 of 3 people found the following review helpful. Enjoyable and informative read By Jonathan Fox Detailed and well-written book about the Fairey Barracuda and how it managed to have an important role in the war effort despite all its flaws. Very enjoyable read recommended to any war and aviation enthusiast. 0 of 0 people found the following review helpful. An MMP Orange Series winner By Jim Davis The latest entry in MMP's Orange Series (this is the 17th book in that series, No 8117) is European A4 sized (8-1/4" x 11-3/4") and is a 168 page, square bound card cover. The subject, the Fairey Barracuda carrier torpedo-bomber-reconnaissance airplane, is a welcome one, not being well covered otherwise. In my own collection, I have the Warpaint from many years ago and the even older Fairey and British Naval Aircraft Putnam volumes which go back much further yet. The aircraft has a poor reputation and the author takes an excessively defensive tone throughout and tries hard to rehabilitate the Barracuda. I think he rather succeeds although he stills comes off as less than objective. By the last page of the book the Barracuda becomes "one of the most successful in the Fleet Air Arm's history" which is surely overstating things a bit. Throughout the book the aircraft is compared to the Grumman Avenger but I think a fairer comparison would have been to the Curtiss SB2C Helldiver. Both aircraft had popular predecessors (Fairey Swordfish and Douglas SBD, respectively) which had to soldier on longer than anticipated because of development problems which were never completely cured. Both performed adequately if not brilliantly but one gets the impression that most of the heavy lifting had been done by that point. A big difference would be that Fairey's reputation did not suffer hardly at all while Curtiss' certainly did. The text is very well done and gives complete coverage of the aircraft's background, design, testing, production, and service history. The latter is especially well written with many former pilots and crew members reminiscences. This part of the text is well supported by black and white photographs and some modern, commissioned line drawings. The latter cover 6 pages and most of the drawings are to 1/48th scale but the upper and lower plan views are to 1/72nd scale for space reasons. The book has an extensive technical description which is hamstrung by the lack of any complete surviving example. The author compensates to some extent with photos of salvaged wrecks which await restoration. I think he should have relied more on period technical literature. As it is, this is largely restricted to cockpit arrangements. The camouflage and markings are depicted on 15 pages of color profiles with a few plans. My other criticisms are fairly minor. The author confuses the reader by referring to flaps and dive brakes as if they were separate components and not one and the same. He also implies that there were fuselage mounted radar receiving antennas but I think this is not correct and they do not appear in any photo or drawings. Also, the author fails to mention that the Barracuda could not have been armed with rockets because the Fairey-Youngman flaps would have been directly in the exhaust of these rockets. But all in all, the book is highly recommended. I enjoy learning more about aircraft where my knowledge is fairly basic and the Fairey Barracuda fits that description perfectly.

The Fairey Barracuda was the first monoplane torpedo bomber operated by the Royal Navy's Fleet Air Arm. The Barracuda experienced a difficult birth and development during the Second World War, and this, added to a number of fatal crashes, led to a poor reputation which the aircraft would never truly shake. Despite this, the Barracuda proved highly successful in service, carrying out raids against the Tirpitz, and against Japanese forces in the East Indies that

contributed greatly to the war effort. It also undertook a variety of less well known roles, and remained in Fleet Air Arm service into the 1950s. This new book by naval aviation historian Matthew Willis contains an extensive history and technical description of the Barracuda, drawing from a wide range of archive materials and accounts from the men who flew and operated the aircraft in service, together with over 100 photographs, many never before published. Scale plans and color profiles also included.

"Highly recommended to aircraft historians and modelers alike." (AMPS Indianapolis) Over 100 of excellent photos many previously unpublished illustrate the account. And many clearly depict aircraft details for modelers Excellent 1:48 and 1:72 scale drawings help fact-check your kit. And 15 pages of color plates provide plenty of project possibilities Robustly recommended! (Cybermodeler)